

# THE DRAYTON VALLEY MUNICIPAL DEVELOPMENT PLAN

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**Executive Summary**

*The Municipal Development Plan demonstrates that the growth of Drayton Valley is contingent on our ability to adapt traditional uses into new realities and effectively manage land.*

*The central core must become a more diversified community hub, with a variety of uses and a focus on street level activity and aesthetics. The quality of design will live up to the standard initiated in the main street plan. Mature residential neighbourhoods must begin revitalizing by converting land for greater housing density without abandoning attractive traditional qualities. New residential areas will have consistent services, varied densities and excellent design qualities. New commercial/industrial areas will be spread along the 50<sup>th</sup> Street and 50<sup>th</sup> Avenue axis as the Town capitalizes on increasing growth and highway traffic. Heavy industrial development will be grouped into the southwest portion of Town to minimize secondary effects. Recreational land and service will continue to receive high priority and be of a consistently exceptional calibre. Infrastructure services must be prepared for both substantial growth and the information age..*

## INTRODUCTION

"Make no small plans for they have not the power to stir the blood."

*Daniel Burnham, Plan of Chicago*

In 1960, Kevin Lynch wrote, "Looking at cities can give a special pleasure, however commonplace the sight may be. Like a piece of architecture, the city is a construction in space, but one of vast scale, a thing perceived only in the course of long spans of time. On different occasions and for different people, the sequences are reversed, interrupted, abandoned, cut across. It is seen in all lights and all weathers.

At every instant, there is more than the eye can see, more than the ear can hear, a setting or a view waiting to be explored. Nothing is experienced by itself, but always in relation to its surroundings, the sequences of events leading up to it, the memory of past experiences. Every citizen has had long associations with some part of his city, and his image is soaked in memories and meanings."<sup>1</sup>

Drayton Valley is more than the passive observer of this phenomenon, we are the active participants. An important part of the creation of an image is the development of planning documents that support, critique and guide our planning decisions. The foremost document of these is the Municipal Development Plan, the long range planning document of Drayton Valley.

The challenge of the Drayton Valley Municipal Development Plan is to aid in the creation of a community which will impact the future forever - a piece of architecture, a character, an image that everyone is proud of.

This MDP is to be a guide for the direction, growth and management of land use within the Town. The reader of this Plan will find policies regarding future land use, the manner of and proposals for future development, the coordination of land use and infrastructure with the M.D. of Brazeau, provision for transportation systems and provision for municipal services and facilities. The MDP must also contain policies regarding sour gas facilities and municipal and school reserves. (Municipal Government Act, s. 632)

More than an administrative handbook, the Drayton Valley Municipal Development Plan is intended to be a document that both reflects on our strengths and inspires us to a new vision. It should act as a means of prompting thoughtful development and careful consideration of patterns of development. It should also act as a mechanism of support for the community's general will and general well-being.



# BACKGROUND



## HISTORY

The first community within Drayton Valley began at about the turn of the century. The first settlers of the Drayton Valley area made their living as either lumbermen or trappers. In 1930, the Anglican Church established a mission at the hamlet of Drayton Valley providing it with a nurse to care for the medical needs of the community. In 1936 the Anglican Church was erected. The economic base in these early times was the lumber industry, which in 1930 employed 300 to 400 bush workers. This forestry, combined with agriculture which was beginning to occur on cleared lands, remained the main source of income until the discovery of oil in 1953.

Just before the discovery of oil only about 75 permanent residents lived in Drayton Valley. The development was then concentrated around the intersection of what is today 50<sup>th</sup> Street and 50<sup>th</sup> Avenue. Two churches, a post office and a two-classroom school were the focus of the community. After the oil boom, the community facilities were outgrown overnight.

Rapid expansion created the usual growing pains and in 1954 the community came under the jurisdiction of the Town and Rural Planning Branch of Alberta Municipal Affairs. The department laid out the new expanded townsite on a modified grid pattern. The commercial core was to be near the older existing portions of the hamlet, industrial would be to the west and residential to the east.

During the first few years the main direction of growth for industries was westward along 50<sup>th</sup> Avenue with later expansion extending northwards along Industrial Road. Residential growth progressed east and then south.

In the 1960's the population began to decline as automation in the oil industry reduced the number of jobs in the Drayton Valley area. Things stabilized in the 1970's as an active, but stabilized, oil industry kept the Town growing a modest rate throughout the decade.<sup>ii</sup> During this period the residential population continued to grow toward the south although plans were being made to extend residential areas east and north.

In the 1980's and 1990's a major shift in the structure of the Town took place due to the construction of what is today the Weyerhaeuser sawmill and OSB plant. The Town's economy and land use plan was greatly altered by this event. The direction of residential growth was adjusted and the Town became less reliant on a single industry and thus far more stable. The community also continued to become more of a regional centre, housing the offices of some provincial services as well as becoming the seat of the new rural Municipal District, Brazeau, which was formed in 1988.

The planning changes brought about by the industrial growth have included a residential shift to the north and east, an expansion of commercial development and a greater focus on the importance of Highway 22. Drayton Valley will expand yet more on all these land uses as its position as a regional centre gets reinforced.

## REGIONAL SETTING

Drayton Valley is located 138 km west of Edmonton on Highway 22 and is approximately one kilometre west of the beautiful North Saskatchewan River. Drayton Valley sits at the heart of the M.D. of Brazeau. The area surrounding Drayton Valley is rich in timber, oil and gas deposits and these natural resources have been mainstays of the Drayton Valley economy. Due to its proximity to the international airport, significant highways, natural resources and tourism areas, Drayton Valley is uniquely positioned to be an attractive choice for businesses and visitors.

Drayton Valley is also growing as an agricultural service centre. The agricultural community around Drayton Valley is expanding due in part to livestock production though the soil conditions around Town are not generally conducive to crop production.

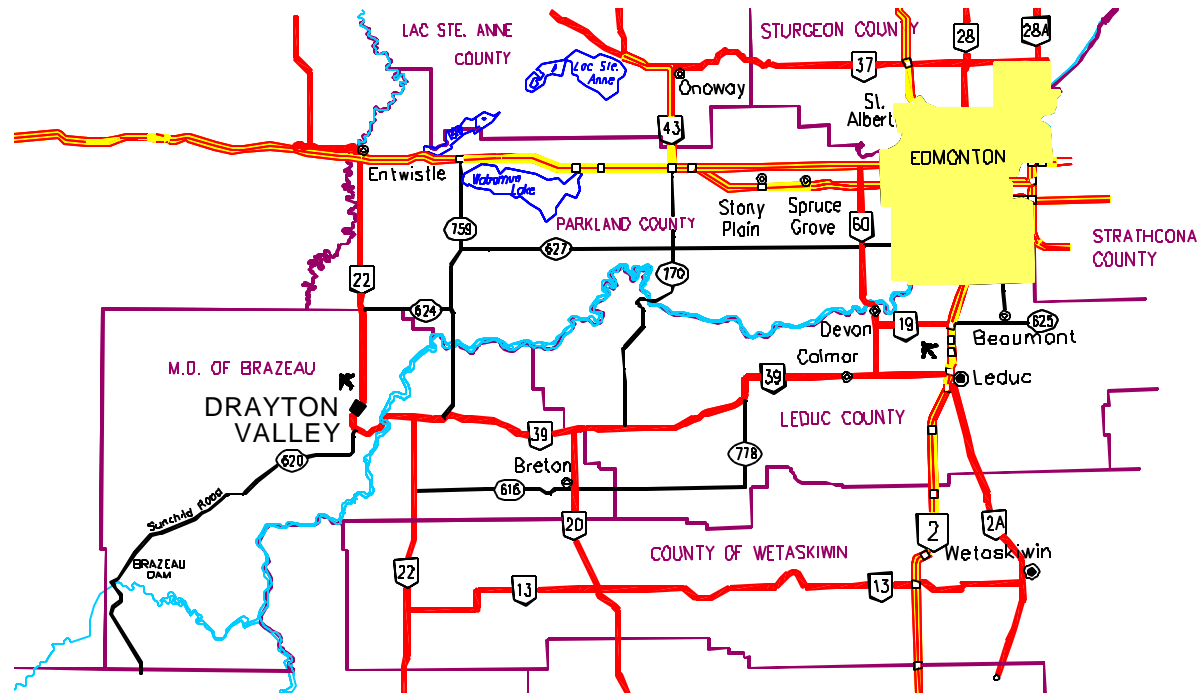
Drayton Valley also functions as a regional government/economic centre to communities within a 50-75 kilometre radius including Breton, Entwistle and many other rural communities.

The highway network around Drayton Valley is very convenient for travel to Edmonton, Calgary, Grande Prairie and

the mountain parks. Highways 22 and 39 connect comfortably with Highways 2, 16 and 43, the three major road linkages in northwest Alberta.

Drayton Valley is also accessible by Secondary Highway 620 (Sunchild Road) which travels to Lodgepole and Brazeau Dam, which, if paved to completion, can cut down travel time to the mountain parks significantly. Drayton Valley has a local airport just north of Town. Drayton Valley is also only 1 hour's drive from the Edmonton International Airport.

The opportunity for growth of industrial, agricultural and economic development is enhanced by the regional setting.

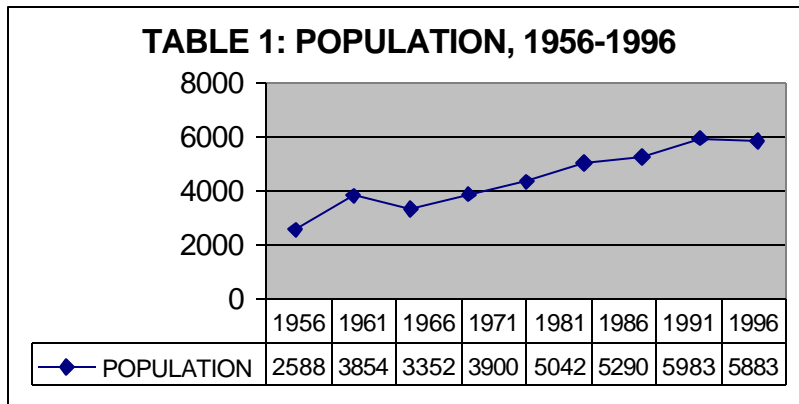


# DEMOGRAPHICS

The purpose of this Section is to provide a general profile of the Town of Drayton Valley. The discussion focuses on information relevant to community planning such as population characteristics, growth patterns, traffic patterns and prospects for future developments.

## POPULATION

One of Drayton Valley's goals is to attain a population of 10,000 which would enable it to consider city status. In 1996 Drayton Valley's official population was 5883, over half the necessary requirement to apply for City status.



Source: Statistics Canada

Despite a reported loss in population between 1991 and 1996 Drayton Valley is unquestionably a growing community. Since 1996 highway traffic counts, residential and commercial

building permits, and subdivision units are well up. Table 1 shows the steady rate at which Drayton Valley grew between the years 1956 and 1996, with "hiccups" in 1966 and 1996. If building permits through 1999 are a reliable indication, the upward population trend will be back on track for the period 1996-2001 and beyond.

## POLICY ISSUES:

**The Plan will need to address a land and service base for future needs which anticipates a population increase of 2.5% per year.**

**Growth is already spilling outside the Town boundaries. In order to provide for a sustained population increase the Town will need to increase its land base.**

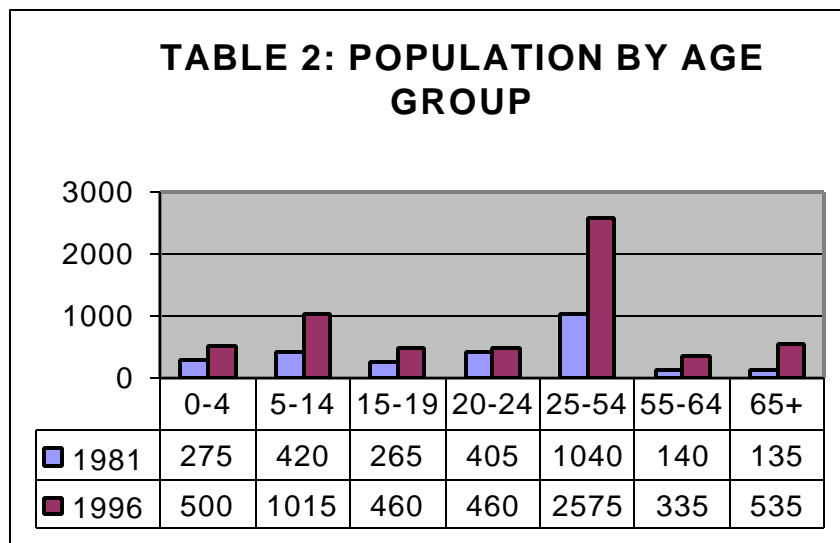
## AGE BRACKETS

Perhaps the more noticeable trend in Drayton Valley is not so much the population growth but the composition of the population, particularly so regarding age. The age of the average resident in Drayton Valley is becoming greater as time passes, which may be a reflection of one or two maturing processes that the entire community has undergone. The first is the transformation from transient oil town to a more diversified, mid-sized regional centre. The second is the development of a dominance of baby boomers in progressively older age brackets.



The group of people 25 and over has risen from 49% of the population to 59% of the population in just 15 years. Additionally, the group of "retirees", those 55 years of age and over has risen from 10% of the population to 15% in the period 1981 to 1996. The total number of citizens 55+ has risen from 275 in 1981 to 870 in 1996, or more than 300%. However, the average age of residents of Drayton Valley in 1996 was 31.3 years of age, still lower than the provincial average of 33.7 years.<sup>iii</sup> Drayton Valley is thus still a community of the young but with an increasing emphasis on the retirement community.

A key will be to have appropriate services in appropriate quantities for all age groups accommodated by the policies of this Plan.



Source: Statistics Canada

**POLICY ISSUES:**

**The Town must continue to provide opportunities, housing and services for youth in order to both keep and draw young residents.**

**The Plan must allow for the development of facilities, activities, parks and districting that are convenient and appropriate for seniors.**

**Drayton Valley's land base should be made suitable for 40 year's growth.**

*BUILDING TYPES*

In the 1996 Census, about 70% of Drayton Valley's 2075 dwellings were listed as "single detached". In the period since that Census, the Town has seen a predominance of medium-high density dwellings built over single detached dwellings.

Since the 1996 Census, developers have subdivided 54 lots under single detached districting and 63 units under duplex or condominium districting.

The majority of these 117 units are already occupied. Only 50% of the newest dwellings in Town are single detached dwellings. The Town is experiencing a shortage of medium density and affordable housing as increasing numbers of youth and seniors look for alternatives to single detached dwellings. Most of Drayton Valley's new duplexes are priced outside the range of the average duplex consumer. As the average household income in Drayton Valley tends to be higher than in

other local communities, the impetus to keep the cost of housing down or to satisfy a lower-income market is reduced.

Exclusionary pricing levels are an issue in Drayton Valley. Although average income levels are quite high in Drayton Valley, a need still exists for suitable lower-income housing. Residents who could otherwise benefit from being located within Town choose to live in surrounding mobile home parks and commuting-distance municipalities, who have more appropriate housing choices for these income levels. Only a small portion of Drayton Valley's housing stock is priced under \$100,000. Choices between \$60,000 and \$100,000 need to be more readily available to draw these additional homeowners.

**POLICY ISSUES:**

**The Plan must balance medium density dwellings throughout the Town through districting policies.**

**Alternatives to single detached dwellings, such as boarders and basement suites, are not readily available at present.**

**Opportunities for housing that is suitable for all income levels will assist in drawing a population base and integrating the community. An absence of such housing contributes to population leakage.**

**The Plan needs to recognize that the largest component of the building sector is still in new single detached dwellings and thus ensure land is available for the same.**

**TRAFFIC PATTERNS**

The activity level of Drayton Valley is also evidenced in the provincial vehicle volume counts for Highway 22 around Drayton Valley. Traffic volumes on the road have fluctuated a bit within recent years but the general trend has been upward.

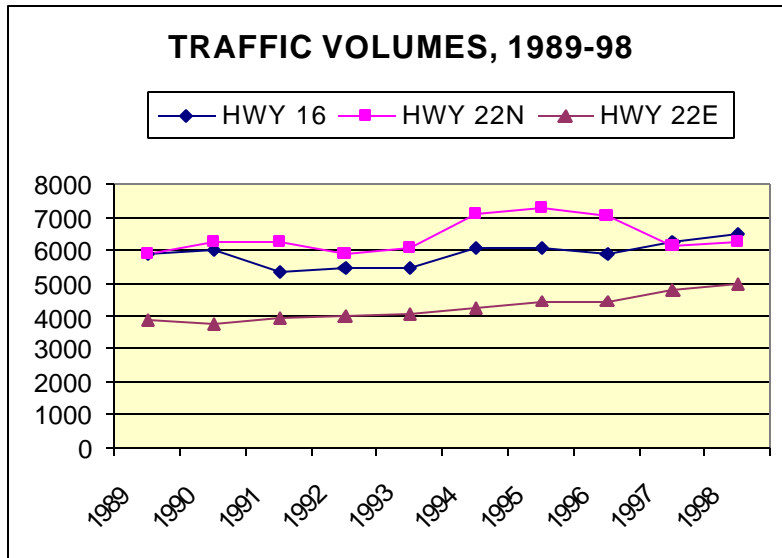
On the south side of Town the volumes are more consistent. The general volume has increased routinely over the past 10 years. Highway 22 on the south, leading to Highway 39, though not twinned, received repairs throughout 1999 and it appears to have become more and more the route of choice to Edmonton and, of course, the International Airport.<sup>iv</sup>

As for the future of a twinned highway at Drayton Valley, Table 3 demonstrates that at selected points, the traffic on Highway 22 north of Drayton Valley has been heavier than that of Highway 16 at Entwistle, which is already twinned with an overpass access.

Better roads not only require upgrades and additional services, but in fact tend to draw travellers who might otherwise not wish to travel to Drayton Valley at all. An upgrade to either Highway 22 or Highway 39, combined with the completion of Sunchild Road (Secondary Highway 620) would have a dramatic impact on travel times to the mountain parks and thus the Town's economic future.

While Drayton Valley is reliant on the Provincial government for improvements to Highway 22, the Town must be proactive in making concerns regarding traffic volumes, bridge

conditions and access management known to the Provincial government.



Source: Alberta Infrastructure

**POLICY ISSUES:**

**The Town must collaborate with Alberta Infrastructure and the Municipal District of Brazeau to ensure that upgrades to the bridge and the twinning of Highway 22 are ongoing priorities.**

**The upgrade of Sunchild Road (Secondary Highway 620) would positively impact Drayton Valley as a tourism hub.**

**The proximity of Drayton Valley to major highways and the International Airport is a commercial draw.**

**The volume of traffic around Drayton Valley makes the access points into Town from the highway a key issue.**

*SUMMARY*

Overall, the indicators of growth seem to be advancing well in Drayton Valley. The growth of Drayton Valley will continue to be dependent on the ability to both keep and draw population to the community. The local housing stock is beginning to diversify and must continue in that direction to supply those sectors of the population which are in need of alternatives to single detached dwellings.

The growth indicators, coupled with a strong economy and healthy oil prices should ensure that Drayton Valley's growth continues into the next millenium. As for predictions on when Drayton Valley might reach a population goal of 10,000 residents, a 2.5% annual increase in population growth would place Drayton Valley's population at 10,000 before the year 2020.

## PEOPLE & SOCIAL NEEDS

The priority of the Municipal Development Plan is, of course, land use - the physical attributes of a municipality's design. However, no development plan is without the influence of the citizens of the community.

The placement of roads, buildings, and industrial developments matter because they lie within our homes, schools and places of work. It is the goal of Drayton Valley to ensure that the Town of Drayton Valley is more than a series of structures. Development issues such as placement of parks, creation of zones specific to industry, design of neighbourhoods and enhancements to business areas all contribute to a greater sense of pride and satisfaction within residents.

This plan is aimed at creating not only a practical and feasible plan for growth. It is also concerned with creating an area in which individuals and families feel a sense of comfort and have continued opportunities for quality leisure and recreational opportunities, irrespective of age, gender or economic standing. Municipal Development Plans in themselves are merely guidelines - the creation of a community is the ultimate goal.

This Plan tries to look at the results of our physical development and address what that translation means to the social life of the community. The Plan will try to develop goals based on the social needs of the community.

The social needs which this MDP seeks to address include:

- the need for variety, reflected in the mix of housing densities, variation among neighbourhood designs, and an enthusiasm for developing on varying building heights;
- The need for equality of opportunity, reflected in opportunities for seniors, children, the economically disadvantaged, through broad access to services, and in the rejuvenation of older portions of Town.
- The need for image-building to be a component of Town operations, reflected in both architectural freedoms and encouragement;
- The need for environmental "productivity", reflected in the preservation of natural areas, the enthusiasm for active outdoor life and programs to encourage pedestrian life;
- The need for planning readiness and the Internet revolution, reflected in infrastructural advances and flexible districting considerations;
- The need for safety and security, reflected in policies aimed at the creation of defensible space and emergency access;
- The need for recreation service, reflected in reserve policies and facilities expansions;
- The need for identification, reflected in legible urban patterns and consistent municipal design qualities.

## GOALS & OBJECTIVES

Drayton Valley has fulfilled much of the promise envisioned in past Plans. Residential areas are developed on lands which once seemed worlds away from Town. The new goals of Drayton Valley sees today include redevelopment, accommodating the growing retirement community, accessing the information age, and the continued commitment to top quality services, design and development.

Revitalization is occurring all over Town. What was once a hockey rink is a civic centre, what was once a restaurant is a senior's centre, what was once a hospital site is an apartment block, and what was once a residential street is now a shopping plaza. The Plan must endorse and support the adaptation of old uses to new growth as a primary goal.

As we saw in the demographics, Drayton Valley's population is growing, but also aging. The shifting demographic of the community is one of the primary underpinnings of Drayton Valley's future. Accommodating the needs of seniors and retirees with effective transportation routes, and appropriate housing will be a primary goal in order to preserve and support Drayton Valley's population base.

The changes in life due to the information age will be another major change. Digital life has already changed the structure of our daily routine. The Internet creates opportunities, but exclusion from it becomes a form of marginalization.<sup>v</sup> The Town must be adaptable and flexible to the results of

technological advancement without losing sight of developing an attractive and balanced community. The Plan must also ensure that Town infrastructure is of superior quality.

Finally, Drayton Valley has to continue to provide the superior quality of services and parks that residents and visitors have come to expect from the community. Public spaces must be of a consistently high quality and will be a source of pride to residents. Additionally, private developments should be of an admirable, modern and innovative design standard.

The primary, overall goal of this Municipal Development Plan is to provide a coherent and consistent vision for Drayton Valley. It wishes to ensure that the visual and effective quality of the Town creates a place people can call home, and a place people are proud of.

### **The Plan's vision of Drayton Valley consists of:**

1. A recognizable town centre with a unique and pedestrian-friendly character.
2. The revitalization and adaptation of mature neighbourhoods to new environments.
3. Consistent and familiar patterns in our respective residential neighbourhoods.
4. Anticipation of growth, land base need, new technologies and preparedness for all of the same.
5. Ecological respect, pride in appearance, and usefulness of park systems.
6. Recognition of our policy strengths and weaknesses and action to correct and highlight each, respectively.

# THE PLAN



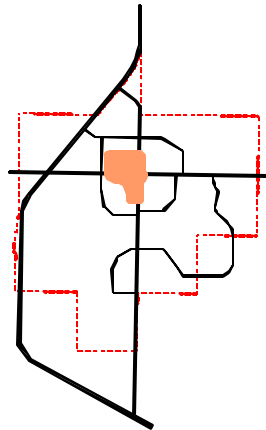
## CENTRAL CORE



The focus of almost every strong community is a strong central core. Downtown is a focal point, a gathering place, an activity centre and a guidepost. In years past, the central core of Drayton Valley may have been limited predominantly to 51<sup>st</sup> Street and some of its adjacent neighbourhood yet most of the activity in Town occurred within this limited

downtown area. As much of the population lived in homes which encircled the downtown, every resident had an opportunity to quickly come and go, meet and mingle, and take advantage of the variety of available services.

The downtown has since seen a great deal of change. The central core of Drayton Valley is in full bloom again. Its dynamism is manifest in the millions of dollars in projects



underway around 50<sup>th</sup> and 51<sup>st</sup> Streets including shopping plazas, condominiums and street level improvements. Its appeal to new arrivals and the local residents is reflected in the active participation that discussions regarding downtown always generate. Downtowns are traditionally a magnet for businesses and tourists and a continuing source of pride for residents. The enthusiasm for municipal ornamentation, from tree-lined medians to ultramodern streetlighting, flower planters to sidewalk improvements, wins admiration from even those who may disagree with the artistic design of the project.



The challenge for downtown now is to develop a unified vision that brings together all the residents of the community. The central core of Drayton Valley has, at times, not received an adequate level of planning attention.

Establishing a balance between the residential and commercial communities has resulted in some public confusion. Drayton Valley will be reinforcing its commitment to downtown by pursuing an Area Redevelopment Plan accompanied by any necessary redevelopment levies. The Plan will bring together the residential and commercial communities, to ensure that all parts of the area receive honest care and attention, that a design focus is re-iterated and that a multitude of uses are accommodated in this unique area to ensure round the clock activity and energy. The effort and style that has been directed to 50<sup>th</sup> Street should be extended to 51<sup>st</sup>.

**POLICIES**

1. The Town will ensure that a variety of uses, including residential, commercial and institutional, are available within the urban core. This will ensure round the clock activity and life within the urban core.
2. The conversion of single family dwellings to businesses will be permitted where a coordinated lot and building design is proposed.
3. The Town will undertake an Area Redevelopment Plan or Design Plan for downtown which addresses:
  - a) architectural design guidelines;
  - b) lot size guidelines;
  - c) parking plans, both on and off-site;
  - d) integration of developments with the 50<sup>th</sup> St. Streetscape Plan;
  - e) signage allowances and requirements, both municipal and commercial;
  - f) traffic patterns, both vehicular and pedestrian;
  - g) on-site landscaping;
  - h) fencing and storage;
  - i) provision for redevelopment levies.
4. Developers are encouraged to develop on multiple floors in order to expand opportunities for retail space, residential lofts, office spaces, and to continue the development of an urban flair and a more intimate scale.

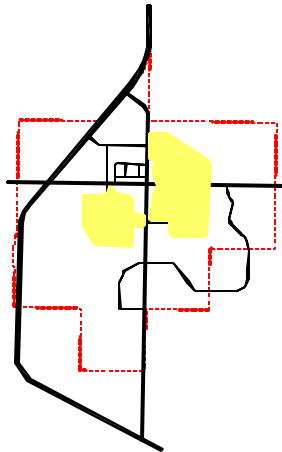


5. Building heights may be kept very liberal downtown. Developers may seek to build tall buildings and/or steep roof pitches where such developments can be safely and effectively serviced by the Town's Protective Services department.
6. Developers are encouraged to consider underground parking to make better use of street level commercial space. Public parking must also be free and clearly designated.
7. Without being disregarded, parking requirements will be secondary to building design and appearance when considering development proposals in the central core. Parking areas should be both unobtrusive and attractive.
8. Downtown parks should be designed to be both community gathering places and rest areas for outdoor activities.
9. Developments in the urban core must be designed to be compatible with the Main Street Plan (*FSC Manasc, 1998*) and should also be compatible with Main Street: A New Direction (*Stauch & Beggs*).
10. Without seriously obstructing pedestrian traffic, restaurants are encouraged to develop patios and businesses are encouraged to use street level space for activities like sidewalk sales to enhance the vibrant nature of downtown's outdoor environment.



## MATURE NEIGHBOURHOODS

The community in the "inner ring" of Drayton Valley contains the small town that the long-time residents remember. The community, adjacent and walking distance to the downtown, contains the traditional vision of a small town; a main street with a selection of stores, a few offices and the Town Hall, access to parks and the community square across from Town Hall. There are houses, some with porches and almost none with attached garages, and facing streets lined with overhanging trees, all layed out in a rough grid design. The scale of the neighbourhood is intimate, almost romantic.



Behind the houses, the community is changing because its demographics are changing. Ads in the paper list the houses for sale as "starter homes" and "fixer-uppers". Mixed into the housing stock is the occasional mobile home, a disappearing remnant of Drayton Valley's transient past. Some of the residents who have stayed are aging and aren't as familiar with their neighbours as they once were. Many of their neighbours have moved into newer condominiums and duplexes around Town. Those who stay still enjoy the convenience - the houses are a manageable size, they can walk to services and stores downtown and the tax bill is relatively reasonable.

Today the mature neighbourhoods need to be physically transformed to better reflect the new population and potential. Traditional design concepts which help provide the neighbourhood with vigorous community spirit may also find favour among developers. It is also conceivable that land values could rise where certain areas are redeveloped along neo-traditional lines.



Policies in the mature neighbourhoods should enhance the compact, friendly character of the community. The percentage of mixed/high density dwelling should be raised to allow for more affordable redevelopment. Additionally, more housing options, such as basement suites, boarders, and home offices can be built into the allowable uses. The mix of uses and identifiable design character are of great importance in sustaining the health of the mature neighbourhood.

**POLICIES**

1. A 20% mix of middle-high density housing should be accommodated within the mature residential area of Drayton Valley.<sup>vi</sup>

2. Community parks shall not be disposed of but shall be maintained and enhanced, responding to the demographic which it serves.



3. Community facilities in mature neighbourhoods shall be designed to be both pedestrian-friendly and handicap accessible.

4. Home occupations shall be encouraged to locate within the mature neighbourhoods. Signage for such home occupations shall be allowable but minimal and unobtrusive to the neighbours.

5. Building designs shall be encouraged to maintain a style consistent with the existing neighbourhood.

6. Garages should be less prominent in mature neighbourhoods. Front porches are encouraged in order to provide both defensible space and as a means of creating architectural character among residences.<sup>vii</sup>

7. Reconstructed houses, however, should be built on two storeys in order to increase living space, provide for home

offices and develop "up-down" duplex potential. Building designs with front attached garages should be discouraged.

8. The keeping of boarders and lodgers and the development of basement suites may be permissible in mature neighbourhoods.

9. Boulevard trees are to be valued and preserved in mature neighbourhoods.

10. Front fences may be allowed where the same are kept low, unobtrusive, designed of natural material such as wood or hedge and are designed ornamentally or for broader neighbourhood continuity.



11. The Town of Drayton Valley will consider installing traffic calming measures (such as curb "bump-outs" or planted islands) where traffic speeds and/or volumes become inappropriate to the area.

12. The Town will maintain consumer choice, competition and growth by ensuring that a healthy number of new residential lots are always available within Town, including by land development when deemed necessary.

## DEVELOPING NEIGHBOURHOODS

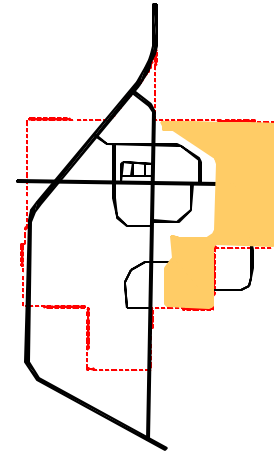
Newly developing neighbourhoods in Drayton Valley grow closer to the Town every day. The vast majority of housing starts in Drayton Valley occur in its newly developing neighbourhoods. These developments are commonly known by names such as "Aspenview", "Northview Gardens", Celebrity Heights", "Glenbow Park" and "Sunrise on the Park". These developing neighbourhoods provide a home primarily to Drayton Valley's middle class.



These communities can be characterized by their differences more than their similarities. The mix of housing densities and styles is typically not varied within these communities. In Aspenview one may find the odd duplex or condominium but the mix is not particularly evident. Madsen Street, on the other hand, provides a number of similar sized duplexes but has no single detached dwellings. Glenbow Park, Celebrity Estates and Northview Gardens are entirely single detached and have no density variety whatsoever. Additionally the latter two are at, or near capacity.

The major attraction of these neighbourhoods is the ability to build a large modern home with all the conveniences inherent in modern architecture. Beyond this, the selling features of these developments tend to be the park system, proximity to

new schools, a modern road network, and the stormwater ponds.





The development of communities in this manner is largely the product of market forces. As a result, these communities tend to be rather exclusive in character.

To alleviate this separation, the Town can require developers to better integrate a variety of housing densities, provide park linkages from neighbourhood to neighbourhood, and ensure that all neighbourhoods have equivalent levels of services and facilities.

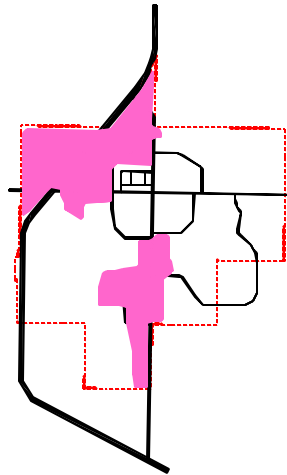
The development of these attractive neighbourhoods in a balanced distribution around Town will be a key component of Drayton Valley's success in attracting new residents.

## POLICIES

1. A minimum 15% mix of alternate housing density shall be required within every Area Structure Plan. An accompanying 10% mix of affordable lot/house sizes shall be included within adopted Area Structure Plans.
2. Council may consider the inclusion of half-acre lots within developing neighbourhoods. Such neighbourhoods shall be serviced with both municipal sewer and water and, though sidewalks may not be required, roads shall be of a paved standard. Streetlighting requirements may also be reduced or discontinued in such subdivisions. 
3. Country residential subdivisions with lot sizes greater than 0.5 acres may be permitted where designed at an approved density within an Area Structure Plan.
4. Primary importance shall be given to the linked park system. Linked parks shall have community facilities and schools as their nodal and/or termination points to ensure the links are continually used.
5. Storm drainage ponds shall be required feature of new Area Structure Plans. A mixture of recreational and natural parks shall be developed throughout Town. Parks should function as focal points for the neighbouring community and shall not be the exclusive domain of neighbouring private landowners.
6. Allowance may be made for one neighbourhood commercial site to be located within each newly developing area (1-2 acres per 160 acres). Permissible uses may include convenience stores, bakeries, florists, hairstylists, medical facilities and similar compatible uses. Non-permissible uses will include gas bars and other vehicle generating or servicing activities. The Town will create a new district particular to this usage. Such activities should be located at the intersection of collector roads and not on local roads.
7. Home occupations may be permitted provided they will not disturb the residential character of the neighbourhood.
8. Developers are encouraged to use boulevard treeplanting, particularly along major collector roads, in order to build a unified appearance to the neighbourhood, create a noise buffer and develop a holding area for snow removal. 
9. Road networking shall be articulated through Area Structure Plans. Dead-end roads shall be discouraged as a fire hazard and school bus problem. Cul-de-sacs shall be used as a tool to manipulate otherwise undevelopable space, shall not be the primary means of developing housing areas, and shall be kept as short as possible for safety. Cul-de-sacs shall not be greater than 75 metres in length.

## COMMERCE AND INDUSTRY

The lines between commerce and industry can often be blurry but the two activities are often so complimentary in Drayton Valley so as to be one entity. It can also be argued that most of the commerce which occurs in Drayton Valley is a function of industry and that most commercial enterprises might be better described as "industrial support".



Planning for such a mix can be a balancing act. For the most part, the restaurants, hotels, professional services and other *commercial* activities live happily beside the auto repair, storage yards and car washes of the *industrial* sector. The negative impact that one activity might have on another is mitigated by the necessity of their interdependence. Drayton Valley has for years reflected this reality through the broad-based use of "C-MX" or "Commercial-mixed" districting

within its Land Use Bylaw. The mix has been very successful in developing commercial areas and will be continued into this new Plan.

Despite this arrangement, some call for change is evident. Part of this will be the implementation of a CBI district which will provide greater aesthetic control on 50<sup>th</sup> Street south. The most pronounced evidence of this is where the Town is most visible to the travelling public, along and at major entrances to Town and at locations where an exceptional standard of design has been set. The Plan contains policies which call for attractive development at major intersections and entrance points.

Commercial development will also continue to stretch itself out along the major 50<sup>th</sup> and 50<sup>th</sup> axis, which grow in prominence every day. Development on 50<sup>th</sup> Street to the south has flourished throughout the past ten years. At the north, a number of new commercial developers have chosen the north 50<sup>th</sup> Street entrance as a prime commercial area. The recent arrival of a McDonald's restaurant in the west has energized the commercial artery along 50<sup>th</sup> Avenue. The Plan will continue this growth by allowing for the broad mix of commercial and industrial support along Drayton Valley's major corridors.



Drayton Valley will ultimately be a city of industry, which will strive to put an attractive face and quality of life parallel to none forward within its industrial roots.

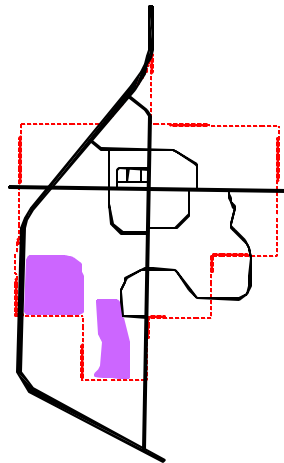
## **POLICIES**

1. Drayton Valley's Land Use Bylaw will be designed to accommodate both commerce and light industry in contiguous and/or side-by-side districts.
2. Restrictions on industry in commercial districts shall occur only at locations of prominence within the community including key Highway accesses. Restrictions on industrial uses may also occur where the Town finds a proposal more suited to a medium/heavy industrial district.
3. The Town will collaborate with the M.D. of Brazeau to create a district compatible to both municipalities on the 50<sup>th</sup> Street Commercial/Light Industry corridor on the south side of Drayton Valley.
4. Industrial storage in mixed commercial/industrial areas should be screened from public view as much as possible. Barbed wire should be used judiciously and kept to as minimal an area as possible.
5. Wherever possible, major entrances to commercial sites should be designed to create complimentary intersections with opposing entrances or roadways.
6. Parking and vehicle requirements should not be relaxed in commercial/industrial districts as the areas will be designed almost exclusively for automobile traffic.
7. Accommodation should be made for the movement of larger trucks on commercial and industrial sites.
8. Handicap access shall continue to be of primary importance within commercial/industrial areas.
9. Environmental Reserves shall be provided as required by the Town of Drayton Valley and should be accompanied by municipal reserve buffers to ensure protection of local watercourses and other natural areas. The Town shall be attentive to ensure that commercial/industrial activity does not encroach onto, or negatively impact, these reserves. Reserves should also be provided as visual buffers and seams to separate incompatible uses (such as residential).
10. The Town will collect parks in commercial/industrial lands through municipal reserves in accordance with the needs of the walkway system conceptual plan as outlined in the Integrated Planning Area Infrastructure Requirements and Coordination (ISL & KPMG, 1999) report.
11. Where the Town deems commercial/industrial reserves unnecessary for parks, municipal reserves owing will be provided as money-in-lieu of reserves.
12. Commercial developments will be required to adhere to landscaping standards in accordance with the Master Tree Planting Plan (EDA Collaborative, 1997) where feasible.

## HEAVY INDUSTRY

The dominant industrial operator in Drayton Valley is the Weyerhaeuser sawmill and OSB facility. The facility has become one of the most dominant physical features of the community. Through secondary effects and traffic patterns, the site affects the highway and larger road network, the nature of parks, the location of adjacent uses and the view of Town. The site is furthermore a workplace to a great number of Drayton Valley's daily commuters.

The changes which have occurred due to the development of a major industrial facility have proved to be a learning experience for all parties. Knowing today what types of effects an industrial facility can have on the Town, we are more prepared than ever before to address industrial development proposals.



Provision for heavy industrial use will be limited to those quarters within the vicinity of the Weyerhaeuser site. Already within this general area, Drayton Valley Power and Cantim International operate heavy industrial activity which tend to have a less noticeable impact on the Town.

Most industrial uses have been kept to the topographically lower lands in the southwest portions of Town. The consolidation of heavy industrial



activity has been an ongoing concern which relates also to intermunicipal planning, as the Town does not have a vast quantity of heavy industrial land remaining in this direction. Coordination of heavy industrial activity with the M.D. of Brazeau will be a key component of the Town's future growth.

Heavy industry can dramatically alter the spatial pattern and characteristic of a community due to its secondary effects, the large land mass generally required, and the traffic pattern it generates. When development proposals arrive, the chief goal will be to integrate these developments into a singular large area with as little impact on other districts as possible.

**POLICIES**

1. The Town will respond to market demands for industrial land as they arrive and attempt to harmonize such development with existing development.
2. Drayton Valley recognizes the importance of heavy industrial activity to its long term growth and viability. With this in mind, the Town will continue to welcome new and diversified industrial projects and will make effort to ensure that lands are available either in Town or the urban fringe for such developments.
3. The Town will continue to consolidate heavy industrial activity into the southwest portions of Town.
4. Entrances to major industrial facilities from Highway 22 shall be spaced apart as required by Alberta Infrastructure and as required by the engineering standards of the Town of Drayton Valley.
5. In the interest of both the industrial facility and the neighbouring landowners, the Town shall make effort to buffer the rest of Town from heavy industrial uses through reserve buffers and progressively "lighter" districting
6. The Town will ensure that the industrial district is protected from encroachment by non-compatible uses by limiting the number of allowable uses within heavy industrial districts.
7. Notwithstanding policy 6 of this section, a limited amount of medium industrial and commercial activity may be allowed within heavy industrial districts where the activities are related to the operations of industrial districts.
8. Environmental reserves shall continue to be collected within industrial subdivisions. The Town will be conscious to protect West Creek and any other environmentally sensitive lands through the use of environmental reserves. Public recreational uses may also be provided along creeks and environmental reserve areas.
9. Municipal reserves may be taken to buffer environmental reserves or to allow separation or screening between differing land uses.
10. During any major development or reorganization of sites, industrial operators will be required to develop and/or maintain fire and emergency protection programs with the Town of Drayton Valley Emergency Services department and such programs will accord with any Disaster Services programs of the Town of Drayton Valley.

## INSTITUTIONAL USES

This section focuses on the amorphous group of services that includes schools, health care, and other semi-public uses.



Drayton Valley has both a public (Wild Rose School Division No. 66) and a separate school board (St. Thomas Aquinas RCSR No. 38) servicing Drayton Valley. Within the public board, Drayton Valley has

three elementary schools (Aurora, Evergreen, Eldorado), one Junior High School (H.W. Pickup) and one High School (Frank Maddock). Each of the schools is operating with a modest excess capacity for students so the need for new schools would require a substantial additional population.

The lone Catholic school is St. Anthony's School which serves all ages of students (kindergarten to grade 12) and is at or near capacity. During the writing of this document, the St. Thomas Aquinas School Board was investigating the possibility of expanding St. Anthony's School. Provincial rulings regarding public/separate joint sharing arrangements may have an impact on any such expansion plans.

The need for new schools is governed by the regional population growth. Due to these facts, though Drayton Valley could theoretically fill its existing borders without need of a

new school site within those borders, an increasing population pattern may necessitate a new school site in the Drayton area.

Drayton Valley is home to an adult learning facility, the Pembina Educational Consortium. The Consortium has constructed a new building downtown and this will become a key component in securing Drayton Valley's growth potential, as it provides the opportunity for some residents to continue their education by remaining in, or moving to, Drayton Valley. The Town hopes the Consortium will continue to broaden the horizons of the services it offers within Drayton Valley.

Drayton Valley's Hospital is located on the southeast side of Town and has served as a draw for other related uses. The STARS air ambulance landing pad is adjacent to the site, an assisted living complex has been developed on the site and private medical facilities have sprung up around it. Access to the hospital is a key consideration of Drayton Valley's transportation network and will be reflected in the policies below.

Drayton Valley is also well supplied with churches and private or semi-private institutions including the Legion, the Elks Hall, the Senior's Centre, private ballet schools and more. These institutions provide the lifeblood of the community through their ability to create gatherings, build relationships, and generate change.

All these facilities are best located in accessible public spaces but within reasonable distance of residential areas. It is important to keep the services evenly distributed around Town.

**POLICIES**

1. The Town will respond to School Board requests for school reserve lands or money-in-lieu thereof by developing a program with each School Board for the equitable distribution of these resources.
2. The Town shall, through the Area Structure Plan process, ensure roads and walkways arrive at all schools in such a manner as to maximize access opportunities for pedestrians and cyclists.
3. The Town will continue to negotiate joint use agreements with each School Boards for shared and reciprocal use of school and municipal lands and facilities.
4. The Town may consider locating recreational and cultural facilities adjacent to school sites where surplus or undeveloped land is available within, or adjacent to, school sites.
5. The Town shall continue to encourage development of a transportation link from Madsen Avenue to Beckett Road in order to provide quick and efficient emergency service from the north and east residential areas to the hospital.



6. Medical and medically-related facilities are encouraged to locate within the vicinity of the hospital where such uses are compatible with all other surrounding land uses.
7. The Town will consider allowance for semi-public and private institutional facilities in residential, commercial and central core areas. Such facilities are best located along arterial and/or collector roads. The impact of parking and hours of operation will be of key importance to decisions regarding redistricting of lands to Institutional use.
8. Taller structures shall not be permitted within the vicinity of the air ambulance pad where it appears such structures may conflict with the safe use of the landing pad.
9. Churches should be located along arterial and collector routes adjacent to residential or commercial areas where suitable parking is provided on-site.
10. Day care services may be developed amongst predominantly residential or commercial lands. Day cares should be located on easily accessible parcels with safe walking access to recreational/parks grounds.

## PARKS AND RECREATION

One of the great benefits to living in an urban community is the opportunity to take advantage of a wide variety of recreational services. The park system in Drayton Valley is generally first class and a point of pride among many of its residents. The linked park system, alluded to in earlier chapters, plus a collection of tot lots, storm drainage ponds and active parks, usually adjacent to, and in cooperation with, the schools, provide the cornerstone to this service.

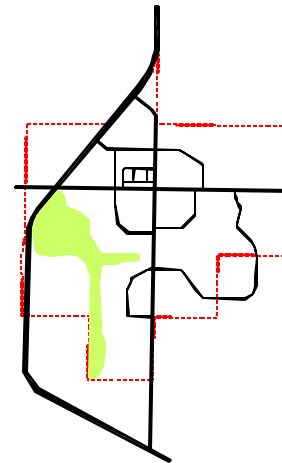
There are, however, many other services which are related to this topic. Foremost among these is the Omniplex, which has become one of the key community gathering points. The Omniplex is home to the ice arena, the curling rink, fitness centre, dance floor, lounge, family games room, trade centre and more. The Omniplex is located near West Valley RV park, rodeo grounds, ball diamonds, and is physically linked to the larger parks system. Additionally, there is ample parking and excellent access to the facility. Maintenance and upgrades to the Omniplex will be of key importance to the Town.



This is not to say that Drayton Valley cannot improve its recreation services. A primary upgrade should begin with the swimming pool. The soccer fields may require provision for

indoor facilities, and a safe, visible skateboard park to allow for spontaneous, unstructured play preferred by the youth.

While active parks and facilities tend to be the domain of the young, Drayton Valley also will provide for the needs of seniors. The outdoor recreational needs of seniors can be addressed by natural areas, wildlife watching sites, as well as



structured outdoor areas such as lawn bowling greens. Additionally, Drayton Valley needs quiet corridors for its bird and wildlife population. As an example, most of West Valley Creek provides such opportunity. Preservation of environmentally significant corridors will be a key component of Drayton Valley's recreational land use.

Part of the Town's success in attracting people to the parks is the combination of superior upkeep and easy identification of public spaces. The colourful, thematic design instills a feeling of security and pride in these spaces and is key to the Plan.

## **POLICIES**

1. Municipal reserves will be collected through the subdivision process to complete the linked park system, develop smaller active parks and buffer environmental reserves. Developers shall be required to outline park dedications within Area Structure Plans.
2. Environmental reserves will be collected through the means provided in the Municipal Government Act. Environmental reserve areas should be kept pristine or used as passive parks, such as West Creek.
3. Storm drainage ponds shall be titled as public utility lots in the subdivision process. Municipal reserves and environmental reserves may be used to buffer these ponds. Public views and access to the ponds should be considered within Area Structure Plans.
4. The Town will not accept well sites and rights-of-way as municipal reserves. They may, however, be titled to the Town as public utility lots. Where feasible, parks can be dedicated beside such areas in order to increase the overall green area and safety buffer.
5. Drayton Valley will make effort to satisfy the parks needs of seniors, making parks accessible to people of all ages and physical ability. Wheelchair accessibility will be considered in park design.

6. Developers should be required to perform a basic level of park preparation when municipal reserves are donated within residential subdivisions. Where municipal reserves are collected by the Town as money-in-lieu, those reserve monies should go into the preparation of parks within the Plan areas from which they were donated.

7. Drayton Valley shall continue to use a coordinated thematic colour pattern to delineate municipal structures and parks.



8. New or expanding recreational facilities should be considered as additions to the Omniplex when feasible. Such projects may include a swimming pool, an indoor soccer facility and a skateboard park.

9. Matching greenery to the surrounding landscape is of primary importance in public areas. The Town will follow the Master Tree Planting Plan (EDA Collaborative, 1997) where feasible.



10. Signage to recreational sites and facilities will be consistent, clear, and illuminated for night-time visibility.

11. Developers are encouraged to maintain as much existing vegetation as possible until such time as lands are subdivided and prepared for development.

## TRANSPORTATION

Drayton Valley's transportation network is highlighted by a classic grid pattern within a series of concentric rings. This chapter seeks to address the need for an identifiable and efficient transportation pattern.

Like many communities, Drayton Valley's dominant roadway is the highway. The highway is the foundation upon which the Town's road network operates. It acts both as a seam in the Town and as a collection point. Highway 22 is the lifeline to Drayton Valley and for this reason it is of primary importance.

Highway 22 has experienced an increasing volume of traffic and congestion on the Highway around Town leads many to the opinion that it should be twinned. As we saw in the demographics, the volume of traffic on Highway 22 around Drayton is comparable to that on Highway 16. Planning for the potential of a twinned highway is seen in Plan policies.

Within Town the network of Drayton Valley's streets is based on the 50<sup>th</sup> Street/50<sup>th</sup> Avenue axis. The collector roads all circulate around this axis and these streets function as the dominant access and egress points from Drayton Valley to Highway 22.

Access from Highway 22 to 50<sup>th</sup> Street has to be both functional and attractive, as these busy points provide the Town's important first impression. The Town will make every

effort to design attractive highway entry points as a joint effort with the M.D. of Brazeau where we have shared access points.

Traffic patterns within Town can get congested toward the intersection of 50<sup>th</sup> and 50<sup>th</sup> and this is compounded by increased levels of activity occurring within and toward downtown. The prominent 50<sup>th</sup> Street streetscape plan designed to slow and calm traffic while improving the timing of streetlights. The Plan anticipates the need to design urban spaces so as to compliment the streetscape design. The 50<sup>th</sup> Street corridor, in general, needs to be widened and given attention.



The dominant collector roads in Drayton Valley include 43<sup>rd</sup> Street and Industrial Road. In the future Madsen Avenue and Beckett Road may become more dominant when the linkage between these two roads is complete. The Town will be cautious about intersection control, access frequency and appropriate widths when collector roads are developed.

One of the most pleasant features of the Town's transportation network is the linked pathway system. More than a recreational tool, they are a valuable element in the larger transportation network, particularly for school children and pedestrian commuters. The parks system must be given detailed consideration in the design of neighbourhoods to ensure it is useful and attractive.

**POLICIES**

1. The Town of Drayton Valley shall maintain a hierarchy and pattern of roadways including arterial, collector and local roads as further delineated in the Integrated Planning Area Infrastructure Requirements and Coordination report.
2. Transportation layouts in Area Structure Plans shall accord with the above report.
3. Access to Highway 22 should be limited to existing access points. The use of service roads and traffic management should be coordinated with both Alberta Infrastructure and the M.D. of Brazeau.
4. The Town endorses the upgrade of Highways 22 and 39 where the same provide access to Highways 16 and 2 from Drayton Valley, including the expansion of Highway 22.
5. Development adjacent to the Highway should be buffered with landscaping consistent with the Master Tree Plan.
6. Consideration will be given to matching road widths with the M.D. where Town roads border or link with the M.D.
7. Without preventing access, fast-wheeled traffic should be slowed and calmed in urban spaces, particularly in front of Eldorado School.

8. The issue of an additional east-west connector, including the possible extension of 34<sup>th</sup> Avenue, should be settled by initiating a traffic study specific to the issue.
9. The Town will continue to maintain a high and consistent standard of boulevard and median treatment. The standard by which designs have been made to 50<sup>th</sup> Street shall be encouraged although the exact design of street furniture and physical structures may vary between neighbourhoods.
10. The impairment of pedestrian traffic is to be discouraged. Pedestrian crossings should be prominently marked and should allow pedestrians to use non-automotive transport throughout all commercial and residential neighbourhoods.
11. Pedestrian paths shall be given equal consideration to vehicle-based routes as components of the transportation network.
12. The Town shall consider traffic-calming measures such as curb "bump-outs" and landscaped islands to discourage short-cutting through local roads.
13. All downtown traffic patterns, access, egress, traffic control, development and landscaping must be coordinated with the 50<sup>th</sup> Street Streetscape Plans.



14. The Town shall continue to require developers to provide land for service roads where necessary along 50<sup>th</sup> Street south of 41<sup>st</sup> Avenue.
15. Town entrances should be designed to be as attractive as possible. Where feasible, the Town will look at expanding the intersections to accommodate multiple classes and lanes of traffic, while adding aesthetic value to the intersections.

## INFRASTRUCTURE

Drayton Valley's infrastructural service includes an extensive water and sewer treatment and collection system. With unique exceptions, Drayton Valley's properties are connected to municipal water and sewer service. Beyond this, water and sewer supply has been extended to some lands within the area bound by the North Saskatchewan River and Highway 22 as well as some lands west of Highway 22 extending north of Poplar Ridge, within the M.D. of Brazeau.

Drayton Valley also operates some facilities which are located within the M.D. of Brazeau including the municipal sewage plant. The regional landfill and airport, both north of Town, are also jointly administered with the Town.

Drayton Valley must work to ensure that its municipal water and sewer services are prepared at all times to meet the service demands of a population of 16,000, as outlined in the Integrated Planning Area Infrastructure Requirements and Coordination report.

### *SMART CITIES*

Another challenge facing Drayton Valley in the future will be to prepare itself for the way in which a municipality does business in the future. Drayton Valley has been pursuing a "Smart Cities" initiative arrived at getting the business and service community in Town travelling down the "Infobahn".

Some of the opportunities at hand include e-commerce, Internet banking, and cyber-commuting. These new ways of doing business will cause us to eventually adapt our customary districting habits. The separation of living space and work space - once thought to belong exclusively in different buildings located in different zones - will not be so severe. All these opportunities will be contingent, however, on having the necessary infrastructure in place.

The current method of Internet connection for most residents is through the phone lines. Telus has the basic infrastructure in place to permit active service.

The current cable supplier is Videon. At present Videon does not provide cable Internet services to Drayton Valley. It should be incumbent on the part of the developers, however, to require cable services to new development in advance of this prospect.

A key concern is supplying quality power, phone, gas and cable service to the Town includes the increasing use of "cabinets" or "pedestals. Though small, the cabinets can be aesthetically unpleasant and thus should either be planned for unobtrusive locations and/or be designed for ornamental value.

Finally, one current planning phenomena in our neighbourhoods is the Canada Post "Supermailbox". These now familiar postal boxes are located at various points throughout residential communities and can be rather sporadic in positioning. The boxes are traffic generators and can cause a parking concern. Accommodation for them should occur throughout the subdivision process.

**POLICIES**

1. The Town shall require that all developments meet the Town of Drayton Valley Engineering Standards and Specifications and any other provincial or federal regulations.
2. The Town requires that all lots of a proposed subdivision be connected to Town water and sewer systems when the lots are developed provided such connection is economically feasible.
3. The Town may require over-sizing of water and sewer mains in any new development areas and may assist the developer to negotiate cost-sharing arrangements with other affected developers or the Town in order to cover the costs associated with such over-sizing.
4. The Town will constantly seek to acquire any federal/provincial funding available for water and sewer facilities upgrades.
5. Outside the Town boundaries, water, sanitary and storm services will be provided as per agreements reached between the Town of Drayton Valley and M.D. of Brazeau.
6. Developers will continue to be required to make financial contributions toward common storm management requirements such as storm retention ponds.

7. Storm retention ponds may be provided at a rate of 1:160 acres in new subdivisions yet are further subject to any Provincial regulations.
8. The Town shall pursue agreements to jointly operate the regional landfill north of Town.
9. The Town shall continue to make recycling of waste a priority in order to minimize the impact on the regional landfill.
10. The Town will strive to coordinate with the M.D. of Brazeau the conversion of the landfill into a parks and recreation area for outdoor activities as per the Master Reclamation Plan attached.
11. Private utilities and Canada Post are encouraged to work with the Town in locating pedestals in new subdivisions to provide aesthetic and ornamental value.
12. The Town will require that cable services be provided to all new planned residential subdivisions regardless of the density at which they are designed.
13. The Town will require that private services in new neighbourhoods be located underground in order to maintain the visual quality of the neighbourhood and the environment.



14. The Town will maintain a standard of water supply storage and distribution to meet domestic, commercial, industrial and fire suppression needs.
15. The Town will encourage developers of older neighbourhoods to install info-technology connections (such as fibre-optics) in order to meet the needs of the "Smart Cities" initiative.
16. Lands which are unsuitable for development due to proximity to water bodies, 1:100 year flood plains, pipeline rights-of-way or oil wells should not be developed without appropriate controls and precautions.

## **AGRICULTURE**

Agriculture is an important component of Drayton Valley's surrounding land usage. While the land around Drayton Valley is not rated highly for crops, it does provide excellent forage land and agricultural activity goes back to times before the oil boom. The absence of a railway station in Drayton Valley, and thus grain elevators, has also largely dictated that most production would also be in livestock.

Agricultural land within the Town boundaries has largely been phased out with the onset of development. Some bare lands toward the south are used for agriculture but activity is highly limited. Drayton Valley's future as an agricultural service centre would be based primarily on agricultural service.

However, active pieces of agricultural activity lie within Drayton Valley's fringe area in the M.D. of Brazeau. The agricultural activity is largely limited to non-intensive livestock, hay land and service industries related to the same. This is a valuable part of the region's assets.

Recently the Alberta government enacted legislation (Farming Practices Protection Statutes Amendment Act, 1999) which intends to protect agricultural operations. Drayton Valley has not been subject to harsh urban/agriculture clashes. However, as the Town grows and expands, the opportunity for this grows. The goal of this section is to address the need for the protection of agricultural operations until lands are ready for development.

## **POLICIES**

1. In accordance with amendments to the Municipal Government Act of the Province of Alberta (which come into force April 29, 2002) the Town shall publish a public awareness advertisement once annually outlining agricultural activity in the larger Drayton Valley area. This advertisement shall be coordinated with the M.D. of Brazeau to ensure fringe lands are addressed.
2. The Town shall respect agricultural operations on existing agricultural lands. While not exempting the need for permits or permitting expansion into perpetuity, lands in Drayton Valley may continue to be used for agricultural purposes until such time as those lands are prepared for subdivision and development.
3. Notwithstanding Section 2, agriculture is not the intended land use pattern within Drayton Valley's lands. This Plan's future land use plan (Map 1) is to be the future dominant land use pattern within Drayton Valley.
4. Outside the Town boundaries, Drayton Valley shall respect agricultural operations and consider the same within its decision making process on neighbouring lands.
5. The Town will consider the preservation of agricultural lands in the policies of any Intermunicipal Development Plan.

## INTERMUNICIPAL RELATIONS

The M.D. is an integral part of the larger Drayton Valley community. The two municipalities are intertwined through services, recreational facilities, shared roads and highways and are developing a new commitment to municipal planning. Any conflicts between the two neighbours have largely been a product of growth and activity, a goals of both municipalities. This circumstance has benefited the fringe area, which has gained a greater level of planning attention from both municipalities.

The municipal fringe is of interest of this document as well. The growth of the commercial/industrial sector, coupled with the growth of a country residential phenomenon, has led to the need for study regarding serviceability, coordinated planning, leapfrogging, and the overall vision for development.

In this light, the Town is undertaking five key steps:

- Negotiation of the annexation of some fringe area lands.
- Joint planning in the fringe area based on one uniform Intermunicipal Development Plan. The IDP should be a document which details a vision of coordinated development for the two municipalities.
- Negotiate agreements between M.D./Town for consistent development and engineering standards.
- Negotiate cost/revenue sharing in the fringe area development.
- Negotiate agreements between the M.D./Town for all shared hard and soft services.

The Town has undertaken policies which attempt to satisfy the aforementioned steps while actively cooperating with the Municipal District of Brazeau in extensive joint planning efforts.

### POLICIES

1. The Town of Drayton Valley will continue to seek a solution to the question of annexation through negotiation with the M.D. of Brazeau.
2. The Town of Drayton Valley will make every effort to complete an Intermunicipal Development Plan with the M.D. of Brazeau by July 2000. The Plan should outline a detailed land use pattern for the fringe lands, a coordinated system of infrastructure development, and a method by which the two municipalities communicate and make decisions.
3. Until such time as an Intermunicipal Development Plan is completed by both jurisdictions, the Town recognizes the *Brazeau Intermunicipal Fringe Plan for Drayton Valley*.
4. The Town of Drayton Valley will continue to refer all applications for subdivision to Brazeau. Similarly, all Area Structure Plans will be forwarded to Brazeau for comment.
5. The Town of Drayton Valley will continue to refer applications for Development Permits for discretionary uses to Brazeau where such applications border or impact

Brazeau. Similarly, land use bylaw amendments, road closure bylaws, and reserve disposal bylaws which border or impact Brazeau will be referred for comments.

6. The Town of Drayton Valley and M.D. of Brazeau share a common Highway. Discussions with Alberta Infrastructure regarding Highway 22 should be clear and unanimous wherever possible, including:
  - a) the twinning of Highway 22 around the Town boundary;
  - b) the development of Sunchild Road (Secondary Highway 620) as a primary highway in order to reduce travel time to the mountain parks;
  - c) the upgrade of the bridge over the North Saskatchewan River on Highway 22.
7. The Town may separately negotiate agreements for its existing water and sewer services.
8. The Town may separately negotiate agreements for its recreation services that are shared with the M.D.
9. The Town may separately negotiate agreements for transportation networks, including shared roads and intersections.
10. The Town encourages the M.D. to require the installation of cable and other infrastructure in new developments to meet the needs of a "Smart Cities" initiative.
11. The Town will approach the M.D. of Brazeau to seek the mutual preparation of a joint master drainage plan.
12. The Town will adopt a 5 year financial plan for the primary service area, which shall be revised from time to time.

## **IMPLEMENTATION**

The Municipal Development Plan will be used by Council to direct Town actions regarding land use planning and development, and to deal with other agencies, groups and individuals on these matters. It serves as a tool indicating what the Town's position on subdivision and development proposals in Drayton Valley should be.

### **POLICIES**

1. This Plan shall be reviewed periodically to ensure that it always adheres to the Municipal Government Act and the Provincial Land Use Policies.
2. This Plan should be reviewed periodically where Intermunicipal Development Plans, Area Structure Plans, Outline Plans, or other planning documents dictate that amendments to the plan are necessary.
3. The Town shall require that larger undeveloped parcels be subject to the preparation of Area Structure Plans. Outline Plans will not be recognized after December 31, 2000.
4. The Town should review this Plan at the conclusion of any annexation application, regardless of the outcome.
5. Any amendment to this Plan shall be done in full consultation with the general public, the M.D. of Brazeau,

and any relevant public or private agency, except where the amendment is of a minor and/or technical nature.

6. The principal means of implementing the land use planning provisions of this document shall be through the Town of Drayton Valley Land Use Bylaw which shall be at all times consistent with this Municipal Development Plan. In the event of a discrepancy between the Land Use Bylaw and this Plan, the Municipal Development Plan shall take precedence.
7. In order to resolve disputes regarding this document or any other planning document of the Town of Drayton Valley, the Town shall at all times maintain a Subdivision and Development Appeal Board.
8. As an alternative dispute resolution mechanism, Town Council or administration may consider the use of mediation to resolve planning disputes provided that all the affected parties are agreeable to such a mechanism.
9. The Town will continue to be cautious about activity within 100 metres of any oil well head. Activity around pipelines used by oil and gas operators in Drayton Valley will heed all Alberta Energy and Utilities Board recommendations regarding excavation, disturbance and crossings. Additionally, Drayton Valley will comply at all times with any requirements, orders and responsibilities of the Alberta Energy & Utilities Board regarding sour gas operations.

## ENDNOTES

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<sup>i</sup> Lynch, Kevin, *The Image of the City* (1960: The MIT Press, Cambridge, MA), page 1

<sup>ii</sup> Makale, Holloway & Associates, *Town of Drayton Valley General Plan*, Edmonton: 1967, page 2

<sup>iii</sup> All population statistics and have been obtained through Statistics Canada's respective Federal Censuses, 1961-1996, available online at <http://www.statcan.ca>

<sup>iv</sup> Transportation volume statistics have been acquired through Alberta Infrastructure as prepared by McElhanny Land Surveys available online at <http://www.gov.ab.ca>

<sup>v</sup> Mitchell, William J., *City of Bits*. (Cambridge, MA: The MIT Press, 1995) available online at [http://mitpress.mit.edu/e-books/City\\_of\\_Bits/](http://mitpress.mit.edu/e-books/City_of_Bits/)

<sup>vi</sup> Nelessen, Anton, *Visions For A New American Dream*. (Chicago: APA Planners Press, 1993)

<sup>vii</sup> Newman, Oscar, *Creating Defensible Space*. (Washington: US Department of Housing and Urban Development) available online at <http://www.defensiblespace.com/book.htm>